

A d d e n d u m

Iowa Department of Transportation
Office of Contracts

Date of Letting: March 19, 2013
Date of Addendum: March 15, 2013

| B.O. | Proposal ID | Proposal Work Type | County | Project Number | Addendum |
|-------------|--------------------|---------------------------------|---------------|--|-----------------|
| 108 | 91-0352-336 | PCC PAVEMENT - GRADE AND NEW | WARREN | BRFIM-035-2(276)65--05-91 IMN-035-2(332)65--0E-91 IMN-035-2(333)65--0E-91 IM-035-2(336)65--13-91 BRFIM-035-2(356)63--05-91 IM-035-2(357)63--13-91 | 19MAR108.A05 |

Notice: Only the bid proposal holders receive this addendum and responsibility for notifying any potential subcontractors or suppliers remains with the proposal holder.

Make the following changes to the PROPOSAL SCHEDULE OF PRICES:

Change Proposal Line No. 0560 2102-0425071 SPECIAL BACKFILL:

From: 8,847.800 CY

To: 9,303.100 CY

Change Proposal Line No. 0680 2111-8174100 GRANULAR SUBBASE:

From: 243,043.000 SY

To: 242,324.800 SY

Change Proposal Line No. 0690 2115-0100000 MODIFIED SUBBASE:

From: 40,154.100 CY

To: 8,013.000 CY

Change Proposal Line No. 0700 2121-7425010 GRANULAR SHOULDERS, TYPE A:

From: 7,668.200 TON

To: 7,689.400 TON

Change Proposal Line No. 0720 2123-7450000 SHOULDER CONSTRUCTION, EARTH :

From: 926.780 STA

To: 950.330 STA

Change Proposal Line No. 0730 2301-1003090 STANDARD OR SLIP-FORM PORTLAND CEMENT CONCRETE PAVEMENT, QM-C, CLASS 3 DURABILITY, 9 IN.:

From: 15,703.300 SY

To: 18,933.400 SY

Change Proposal Line No. 0740 2301-104115 STANDARD OR SLIP-FORM PORTLAND CEMENT PAVEMENT, QM-C, CLASS 3I DURABILITY, 11.5 IN.:

From: 232,606.600 SY

To: 241,659.400 SY

Change Proposal Line No. 0760 2301-9090000 QUALITY MANAGEMENT - CONCRETE (QM-C):

From: 75,923.800 CY

To: 81,930.100 CY

Change Proposal Line No. 0770 2304-0100000 DETOUR PAVEMENT:

From: 27,380.600 SY

To: 29,080.700 SY

Change Proposal Line No. 1420 2510-6745850 REMOVAL OF PAVEMENT:

From: 152,324.400 SY

To: 153,938.000 SY

Add Proposal Line No. 0761 2303-0143503 HOT MIX ASPHALT MIXTURE (3,000,000 ESAL), SURFACE COURSE ½ IN. MIX, FRICTION L-3; 169.000 TON

Add Proposal Line No. 0762 2303-0142500 HOT MIX ASPHALT MIXTURE (3,000,000 ESAL), INTERMEDIATE COURSE, ½ IN. MIX; 437.000 TON

Add Proposal Line No. 0763 2303-0101000 HOT MIX ASPHALT MIXTURE, WEDGE, LEVELING OR STRENGTHENING COURSE; 158.000 TON

Add Proposal Line No. 0764 2303-0245828 ASPHALT BINDER, PG 58-28; 46.000 TON

Add Proposal Line No. 1757 2214-5145150 PAVEMENT SCARIFICATION; 104.000 SY

Add Proposal Line No. 1758 2528-3800000 MODULAR GLARE SCREEN SYSTEM; 45,262.500 LF

If the above changes are not made, they will be made as shown here.

Make the following changes to plan IM-035-2(336)65--13-91:

1. Add Typical SS2.
2. Add Typical RAMP DETOUR.
3. Replace sheets B.3 and B.4.
4. Sheet B.5: Typical 1RP_PP: Replace all 12" entries in the T column with 11.5".
5. Add reference notes for the following items:

Item 3: Also includes 372 cy for Typiclas Ramp Detour and SS2 and 83.3 cy for work on sheet U.10.

Item 17: Also includes 21.1 tons for Ramp B detour. Refer to Typical Ramp Detour.

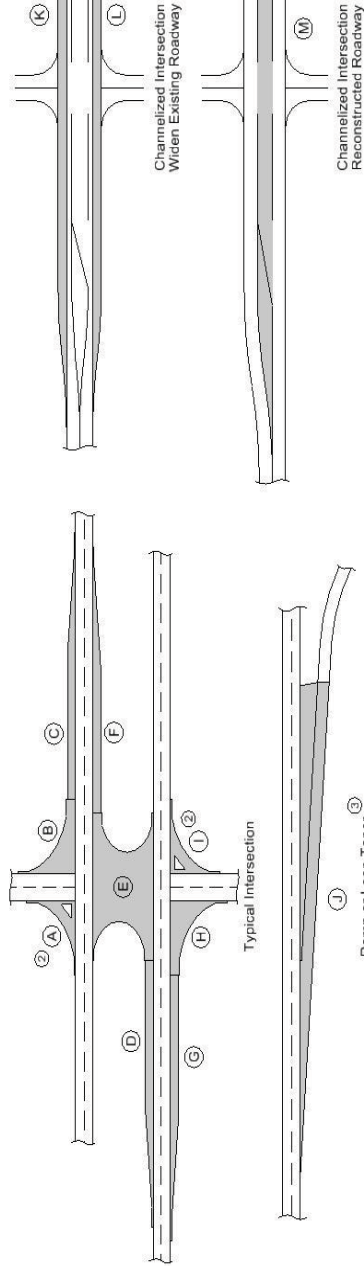
Items 19 and 24: Also refer to Typical SS2 and Ramp Detour.

Item 89: Also includes removal of detour shown on Typical Ramp Detour. Leave inside shoulder for ramp.

6. Replace Tab. 100-24 with the new tab. 100-24
7. Add new sheets: F.1, F.2, K.13, K.14, U.10
8. Sheet J.1: Remove statement : “During construction at ramps at the IA 5 interchange, ramps may be closed between the hours of 10:00 pm and 5:00 am.
9. Sheet J.2: In Stage 2A Section, under construction, add statement: “Construct detour for IA 5 WB to IA 35 SB entrance ramp.
10. Sheet J.23: Replace with Sheets J.23X, J.23Y, and J.23Z
11. Replace J.24, J.25, J.26, and J.27

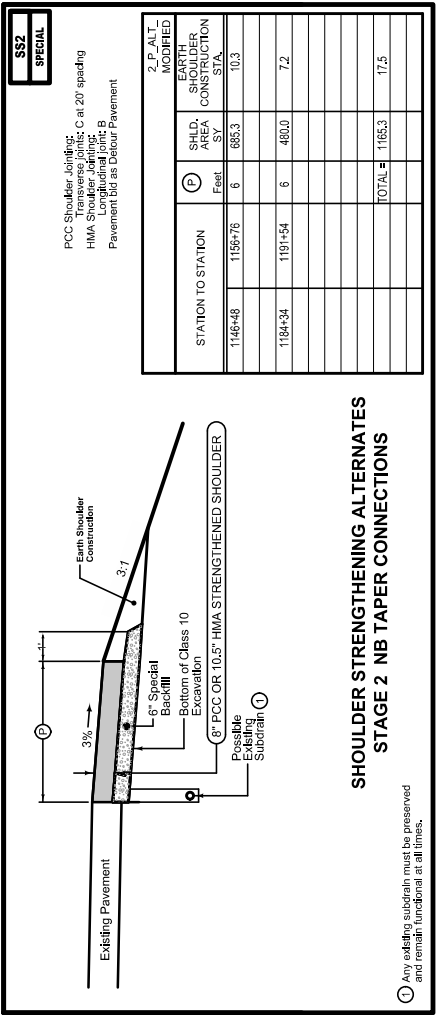
Add the attached sheets and typical:

TABULATION OF PAVEMENT



- ① Quantity Includes Pavement Header.
- ② Does not include Island area.
- ③ Refer to PV-410, PV-411, PV-412, and PV-414.
- ④ Curb area included in areas A and I. Refer to PV-020.

| Road Identification | Station to Station | Width FT | Length FT | Area SY | Area (1) | | | | | | | | | | | | | Subbase | | Remarks | |
|---------------------|--------------------|-------------|--------------|------------|----------|--------|--------|------|------|--------|--------|------|------|--------|--------|------|------|---------------------------|---------------------------|---------|--|
| | | | | | Area (1) | | | | | | | | | | | | | Modified Subbase CY | Granular Subbase SY | | |
| | | | | | A | B | C | D | E | F | G | H | I | J | K | L | M | | | | |
| | | | | | 9 IN | 11X IN | 11X IN | 9 IN | 9 IN | 11X IN | 11X IN | 9 IN | 9 IN | 11X IN | 11X IN | 9 IN | 9 IN | 11X IN | | | |
| I-35 SB | 1088+00.00 | 1145+00.20 | 48.0 | 5700.2 | 30119.0 | | | | | | | | | | | | | 30419.0 | 34220.8 | | |
| | 1145+00.20 | 1157+30.00 | 36.0 | 1229.8 | 4919.2 | | | | | | | | | | | | | 7630.1 | 8448.7 | | |
| | 1157+30.00 | 1184+50.00 | 48.0 | 2680.0 | 13019.3 | | | | | | | | | | | | | 4163.3 | 1959.2 | | |
| | 1184+50.00 | 1195+50.00 | 36.0 | 660.0 | 2100.0 | | | | | | | | | | | | | 1763.6 | 3723.2 | | |
| | 1195+50.00 | 1257+54.41 | 48.0 | 6204.4 | 33899.2 | | | | | | | | | | | | | 33899.2 | 37233.2 | | |
| | 1257+54.41 | 1265+00.00 | 48-60 | 745.6 | 4487.4 | | | | | | | | | | | | | 4487.4 | 4984.4 | | |
| | 1265+00.00 | 1296+02.81 | 60.0 | 3182.8 | 21394.3 | | | | | | | | | | | | | 21394.3 | 22907.8 | | |
| | 1296+02.81 | 1309+12.81 | 48.0 | 1230.0 | 6566.0 | | | | | | | | | | | | | 9271.2 | 9689.4 | | |
| | 1309+12.81 | 1345+30.00 | 60.0 | 3617.2 | 23957.4 | | | | | | | | | | | | | 23957.4 | 25148.6 | | |
| I-35 NB | 1148+00.00 | 1154+00.00 | 60.0 | 600.0 | 1382.8 | | | | | | | | | | | | | 1734.2 | 2134.6 | | |
| | 1154+00.00 | 1166+04.53 | 10.0 | 1244.5 | 1382.8 | | | | | | | | | | | | | 1382.8 | 2212.4 | | |
| | 1166+04.53 | 1186+09.32 | 10.0 | 691.7 | 768.5 | | | | | | | | | | | | | 768.5 | 1229.6 | | |
| | 1186+09.32 | 1199+25.00 | 6-24 | 1230.0 | 2882.0 | | | | | | | | | | | | | 2655.7 | 3475.1 | | |
| | 1252+00.00 | 1265+00.00 | 6-24 | 1260.0 | 2882.0 | | | | | | | | | | | | | 2882.0 | 2591.9 | | |
| | 1265+00.00 | 1308+00.00 | 60.0 | 400.5 | 13194.1 | | | | | | | | | | | | | 2560.0 | 3511.7 | | |
| | 1308+00.00 | 1314+65.08 | 60.0 | 680.0 | 3724.1 | | | | | | | | | | | | | 5504.8 | 5811.7 | | |
| | 1314+65.08 | 1334+50.00 | 60.0 | 1985.0 | 13233.3 | | | | | | | | | | | | | 13233.3 | 14043.3 | | |
| | 1334+50.00 | 1345+30.00 | 48.0 | 1079.9 | 5759.6 | | | | | | | | | | | | | 5759.6 | 6122 | | |
| Paved Median | 1278+09.48 | 1345+30.00 | | | 7221.6 | | | | | | | | | | | | | 7221.6 | | | |
| Overlays | 1345+30.00 | 1356+11.00 | 24.0 | 1081.0 | 2882.7 | | | | | | | | | | | | | 2882.7 | | | |
| I-35 NB | 1345+30.00 | 1356+11.00 | 24.0 | 1081.0 | 2882.7 | | | | | | | | | | | | | 2882.7 | 520.5 | | |
| Ramp 'A' | 1570+68.95 | 1580+50.00 | 1881.1 | 3013.8 | 3066.8 | | | | | | | | | | | | | 4307.2 | 919.3 | | |
| Ramp 'B' | 2554+00.00 | 2573+50.00 | 1919.9 | 3666.8 | 3009.2 | | | | | | | | | | | | | 4026.7 | 803.3 | | |
| Ramp 'C' | 3557+26.66 | 3573+26.28 | 1597.6 | 2837.9 | 245.6 | | | | | | | | | | | | | 3374.5 | 742.7 | | |
| Ramp 'D' | 4571+06.16 | 4586+06.34 | 1590.2 | 2824.6 | 248.5 | | | | | | | | | | | | | 3496.6 | 765.3 | | |
| G-14 | 2167+47.00 | 2157+31.25 | 24.0 | 674.3 | 2658.0 | | | | | | | | | | | | | 2658.0 | | | |
| | 2157+31.25 | 2167+50.00 | 24.0 | 565.0 | 2093.9 | | | | | | | | | | | | | 2284.0 | 464.3 | | |
| | 2167+50.00 | 2181+08.36 | 40.0 | 1912.6 | 8500.3 | | | | | | | | | | | | | 8500.3 | 1565.2 | | |
| | 2181+08.36 | 2191+10.86 | 40-24 | 912.5 | 3244.4 | | | | | | | | | | | | | 3905.8 | 767.0 | | |
| | 2191+10.86 | 2196+75.00 | 24.0 | 564.1 | 1504.4 | | | | | | | | | | | | | 1504.4 | 313.8 | | |
| | | | Totals | | 18933.4 | | | | | | | | | | | | | 18933.4 | 8013.0 | | |
| | | | Qty-C | | 47351.3 | | | | | | | | | | | | | 47351.3 | | | |



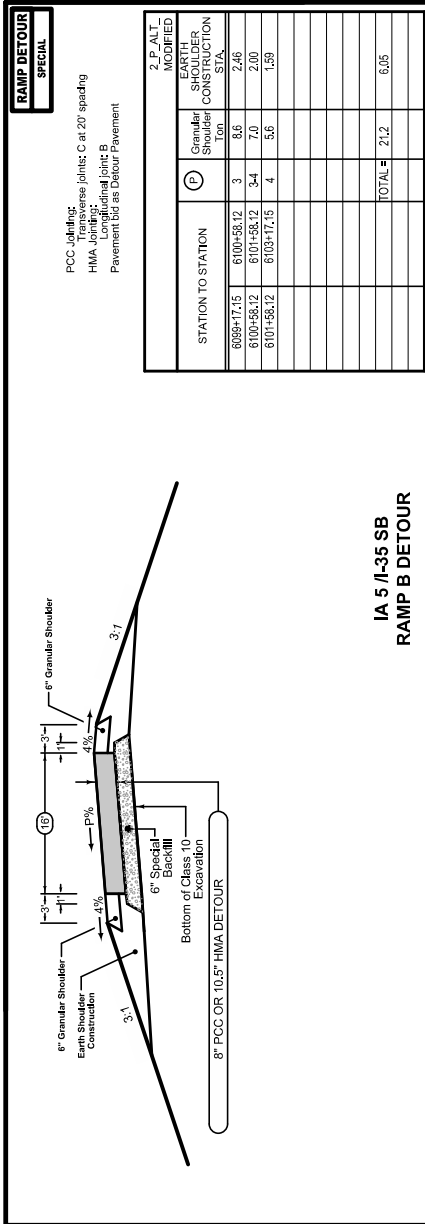
SS2
SPECIAL

PCC Shoulder Joining:
Transverse Joints: C at 20' spacing
HMA Shoulder Joining:
Longitudinal Joints: B
Pavement Bid as Detour Pavement

| STATION TO STATION | Feet | SHLD. AREA SQ. YDS. | 2 P. ALT. MODIFIED | |
|--------------------|---------|---------------------|-----------------------------|------|
| | | | EARTH SHOULDER CONSTRUCTION | STA. |
| 1146+48 | 1158+76 | 6 | 885.3 | 10.3 |
| 1184+34 | 1191+54 | 6 | 480.0 | 7.2 |
| | | TOTAL = | 1165.3 | 17.5 |

**SHOULDER STRENGTHENING ALTERNATES
STAGE 2 NB TAPER CONNECTIONS**

① Any existing subdrain must be preserved and remain functional at all times.



RAMP DETOUR SPECIAL

PCC Jointing:
 Transverse joints: C at 20' spacing
 HMA
 Longitudinal joint: B
 Pavement bid as Detour Pavement

| STATION TO STATION | ① | Granular Shoulder Construction | 2 F ALT MODIFIED EARTH SHOULDER CONSTRUCTION STA. |
|--------------------|-----|--------------------------------|---|
| 6100+47.15 | 3 | 8.6 | 2.46 |
| 6100+58.12 | 3.4 | 7.0 | 2.00 |
| 6101+58.12 | 4 | 5.6 | 1.59 |
| TOTAL = | | | 6.05 |

IA 5 /I-35 SB
 RAMP B DETOUR

ENGLISH

3/15/2013

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DESIGN TEAM

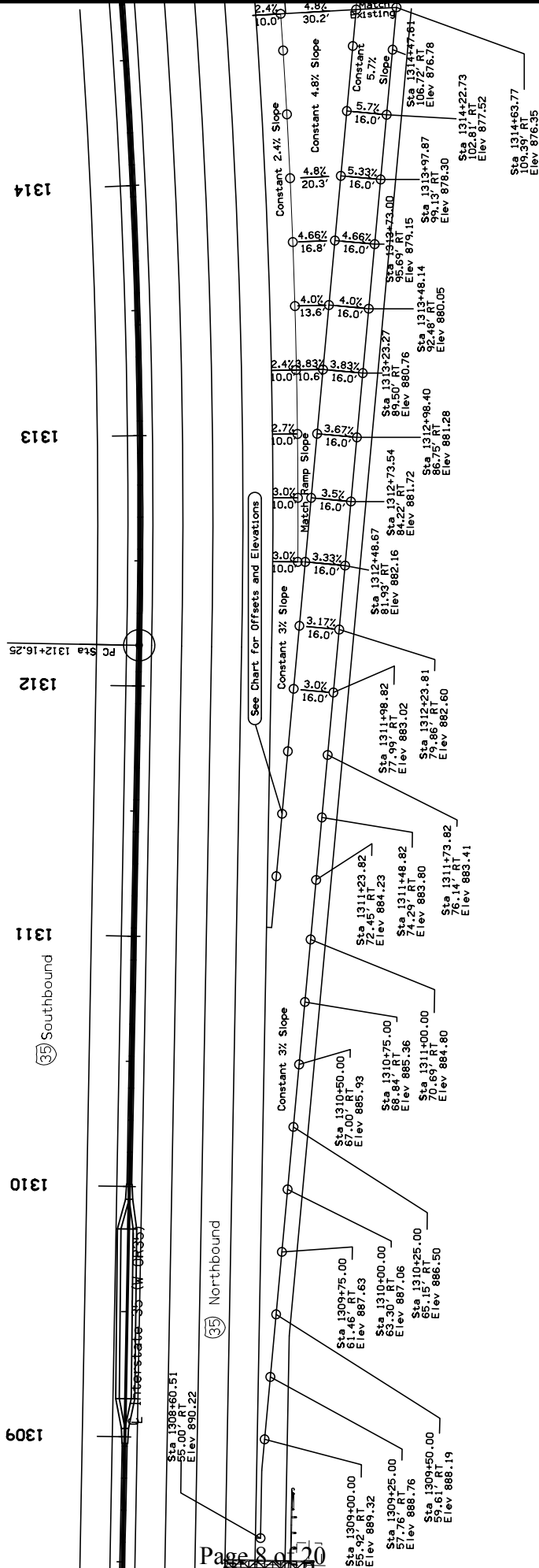
COUNTY

PROJECT NUMBER

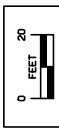
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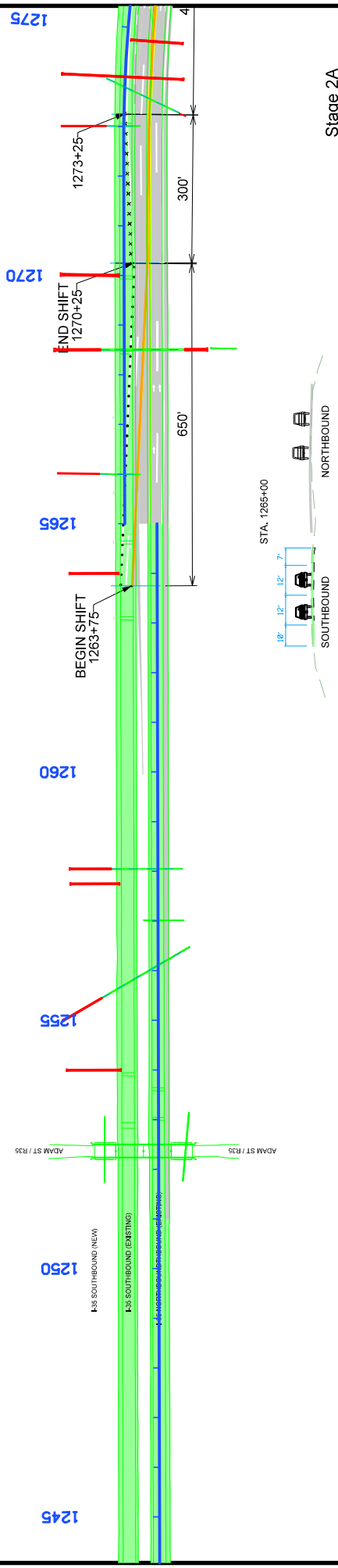
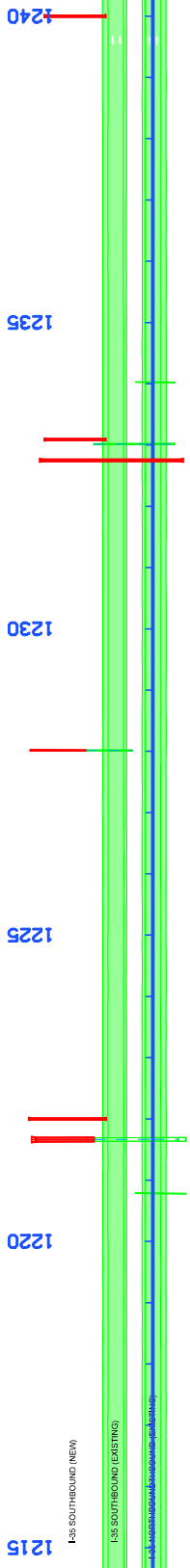


| Station | 1311+25.00 | 1311+50.00 | 1311+75.00 | 1312+00.00 | 1312+25.00 | 1312+50.00 | 1312+75.00 | 1313+00.00 | 1313+25.00 | 1313+50.00 | 1313+75.00 | 1314+00.00 | 1314+25.00 | 1314+50.00 | 1314+64.46 |
|-----------|------------|------------|------------|------------|------------|------------|------------|------------|------------|------------|------------|------------|------------|------------|------------|
| Offset | | | | | | 63.00 | 63.00 | 63.00 | 63.00 | 63.00 | 63.00 | 63.00 | 63.00 | 63.00 | 63.00 |
| Elevation | 56.51 | 58.36 | 60.21 | 62.05 | 63.93 | 66.01 | 68.31 | 70.85 | 73.62 | 76.61 | 79.84 | 83.30 | 87.00 | 879.03 | 878.71 |
| Offset | | | | | | | | | | | | | | 90.93 | 93.23 |
| Elevation | 884.72 | 884.28 | 883.89 | 883.50 | 883.10 | 882.70 | 882.28 | 881.86 | 881.37 | 880.69 | 879.90 | 879.16 | 878.43 | 877.69 | 877.26 |



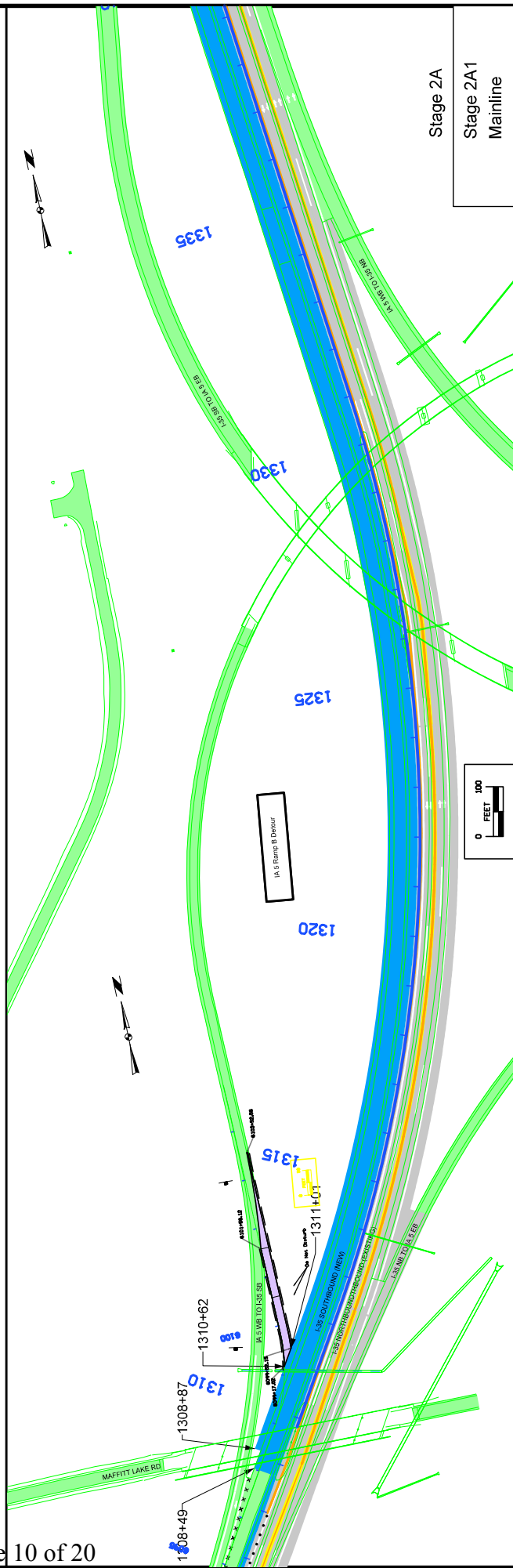
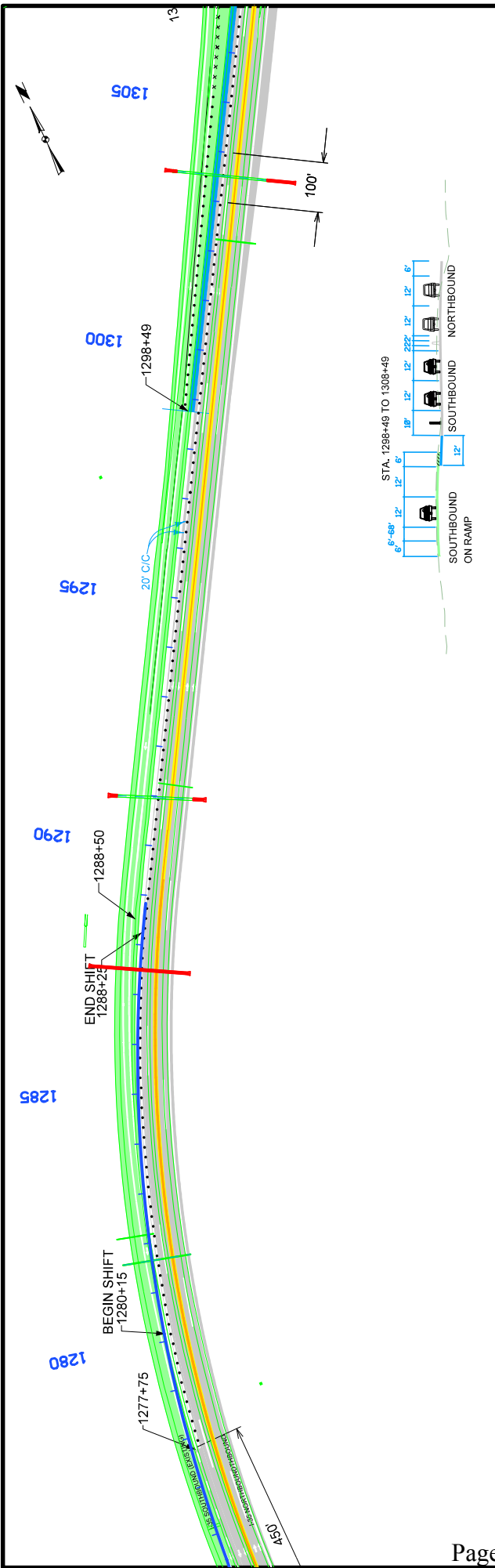
Staking Details
 1-35 and IA 5
 Ramp " D "
 Northbound Exit



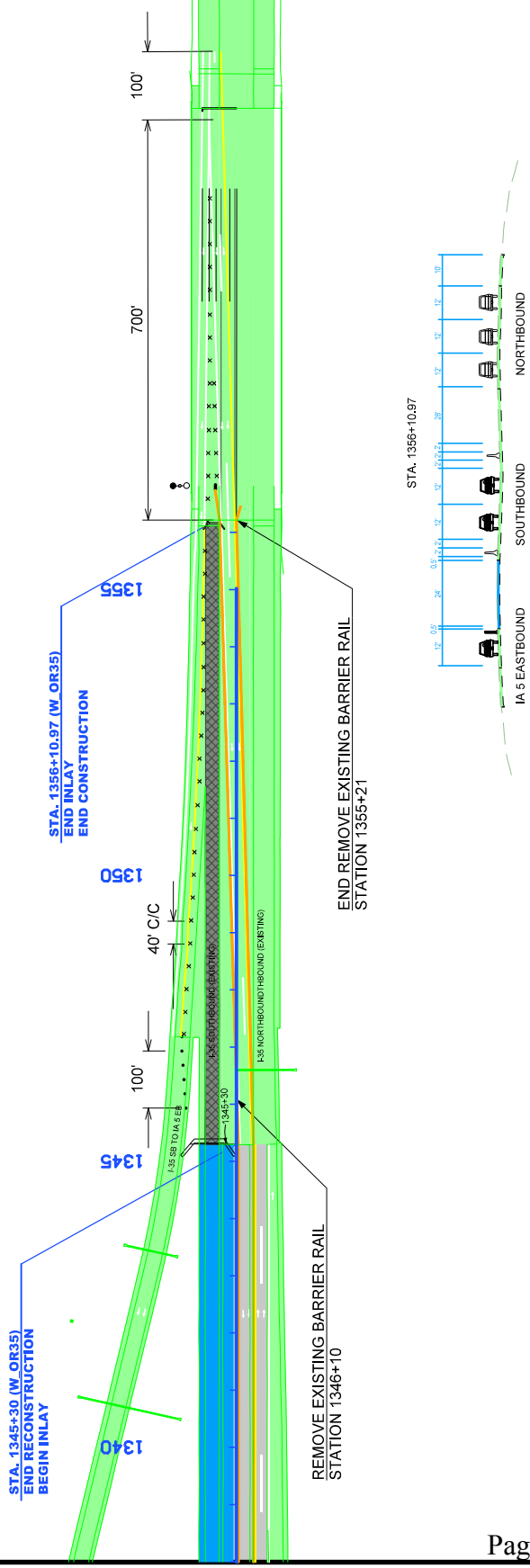


Stage 2A
Stage 2A1
Mainline

| | | | | |
|--------------------|----------------|------------------------|--------------|-------|
| WARREN/POLK COUNTY | PROJECT NUMBER | IM-035-2(336)65--13-91 | SHEET NUMBER | J.23X |
| DESIGN TEAM | | Jia Hoakins\Smith | | |



| | | | | | | | |
|---|-----------|-------------------|--------------------|--------------------|------------------------|--------------------|--------------------|
| ENCL 01 | DATE | DESIGN TEAM | WARREN/POLK COUNTY | PROJECT NUMBER | IM-035-2(336)65--13-91 | SHEET NUMBER | J.23Y |
| C | 3/15/2013 | Jia Hoakina/Smyth | WARREN/POLK COUNTY | WARREN/POLK COUNTY | WARREN/POLK COUNTY | WARREN/POLK COUNTY | WARREN/POLK COUNTY |
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Stage 2A
 Stage 2A1
 Mainline

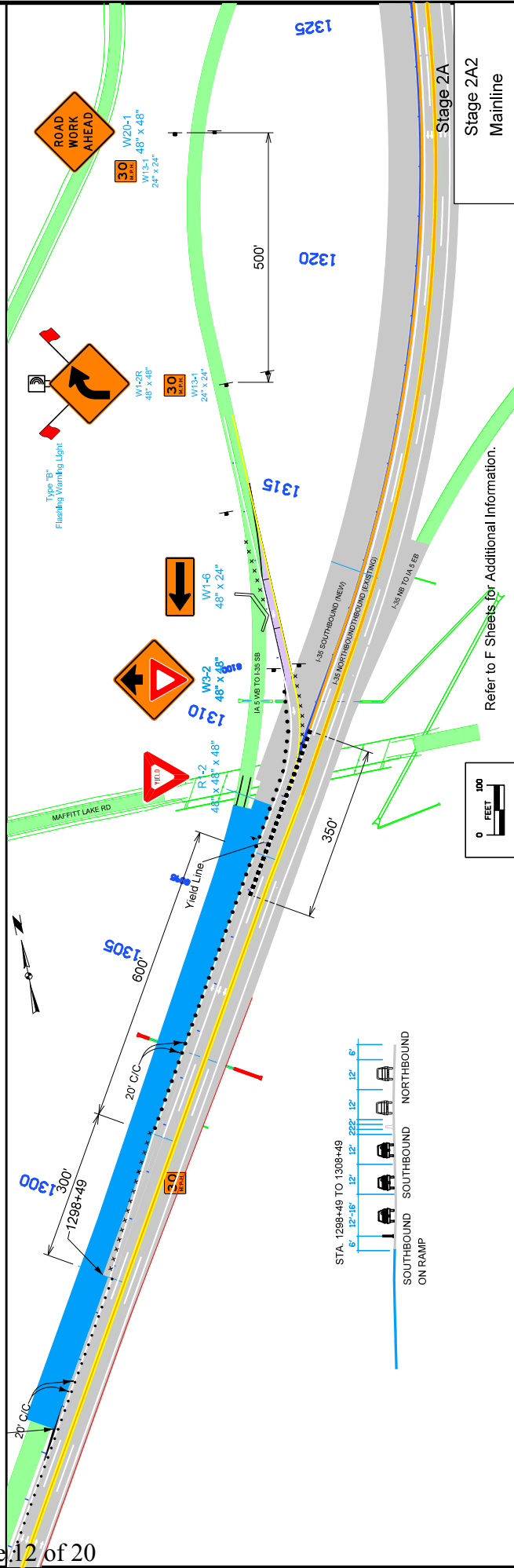
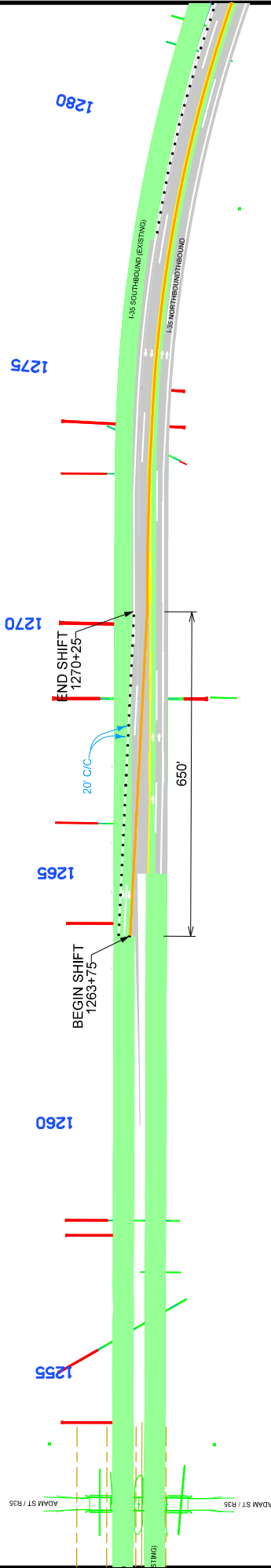
SHEET NUMBER **J.23Z**

PROJECT NUMBER **IM-035-2(336)65--13-91**

WARREN/POLK COUNTY

DESIGN TEAM **Jia Hoskins Smyth**

ENGLIN 3/15/2013 asmyth W:\Projects\PO22007\103503000\Design\1\Sheet_Files\1035536_J02.sht



Refer to F Sheets for Additional Information.

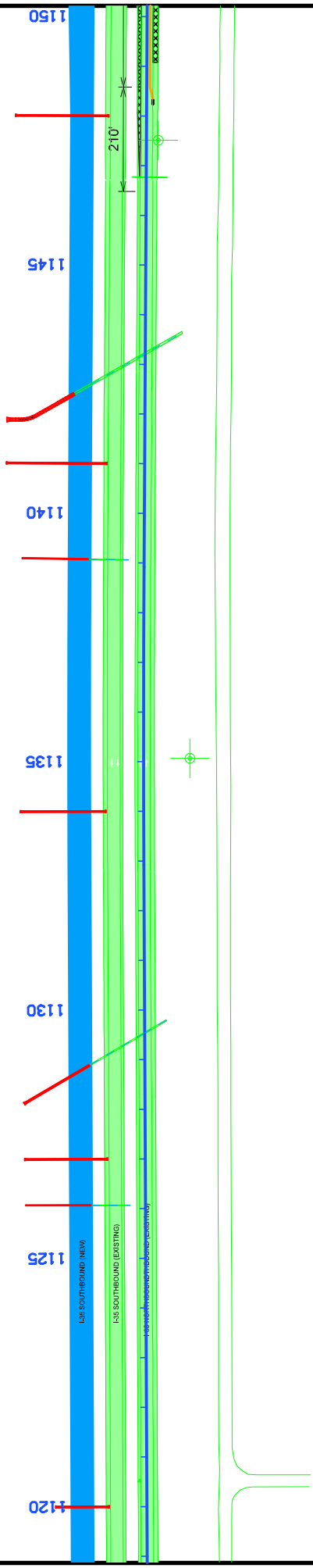
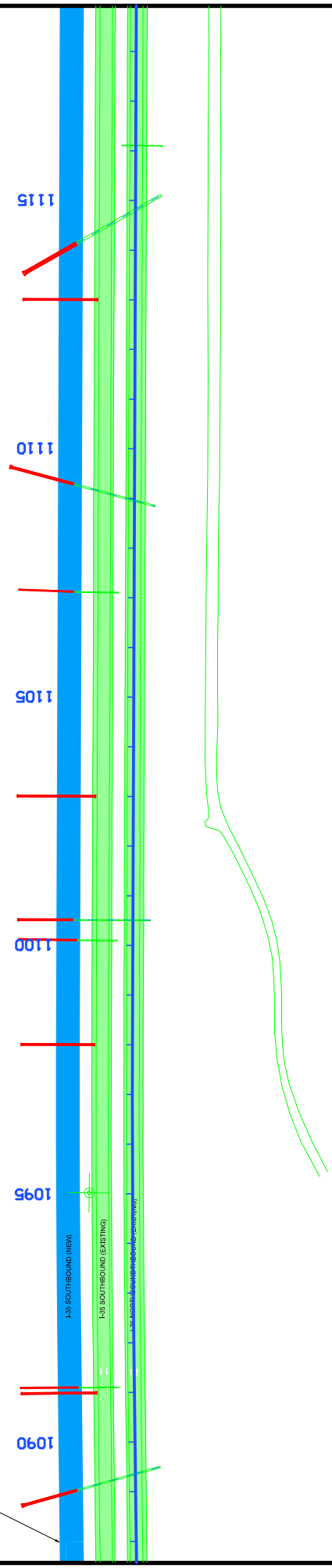
Stage 2A
Stage 2A2
Mainline

| | | | | |
|-----------|-------|------------------|---|--------------|
| ENCLIN | DATE | DESIGN TEAM | PROJECT NUMBER | SHEET NUMBER |
| 3/15/2013 | asmyn | Jia Hoakins\Smth | WARREN/POLK COUNTY IM-035-2(336)65--13-91 | J.24 |

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End Badger Creek Paving Project (By Others)
Begin Cumming Interchange Project
Sta. 1088+00



Stage 2A

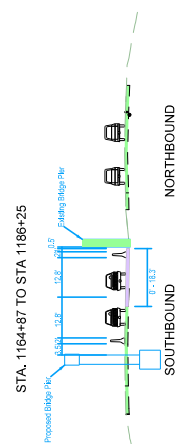
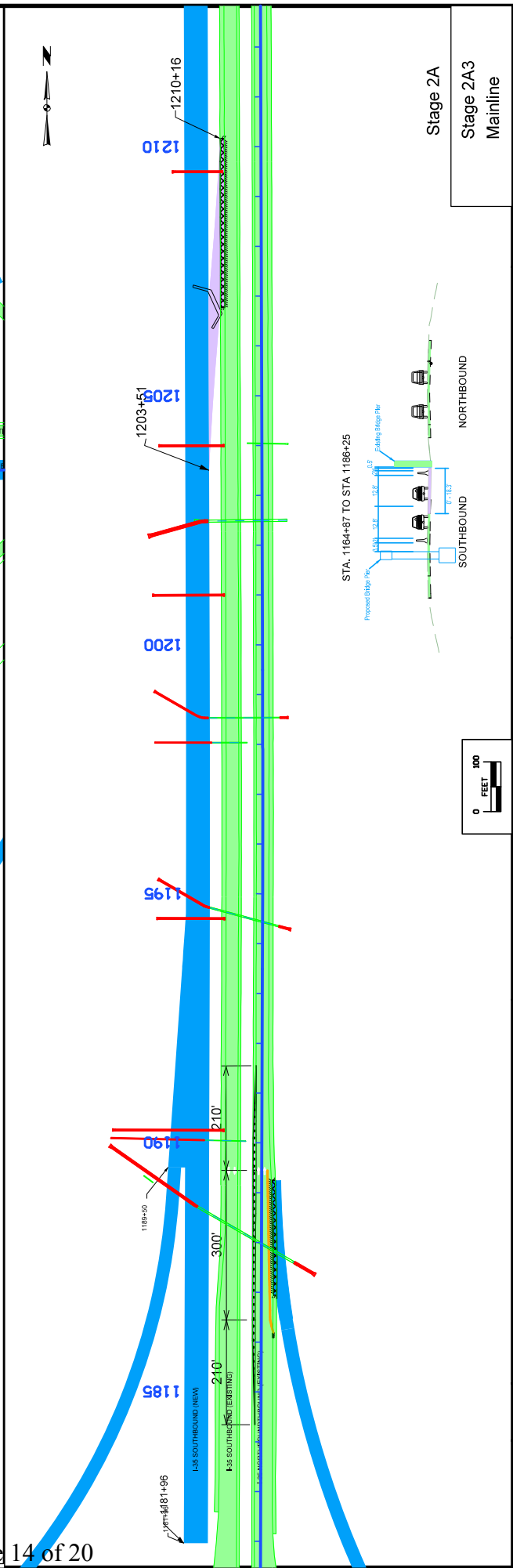
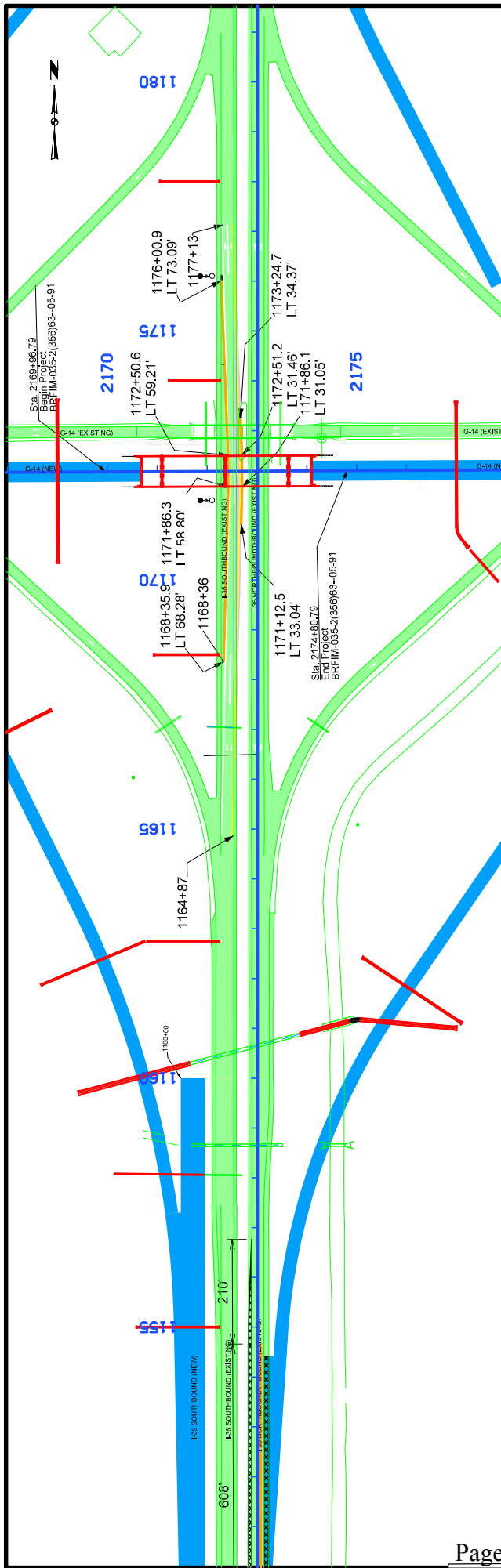
Stage 2A3

Mainline

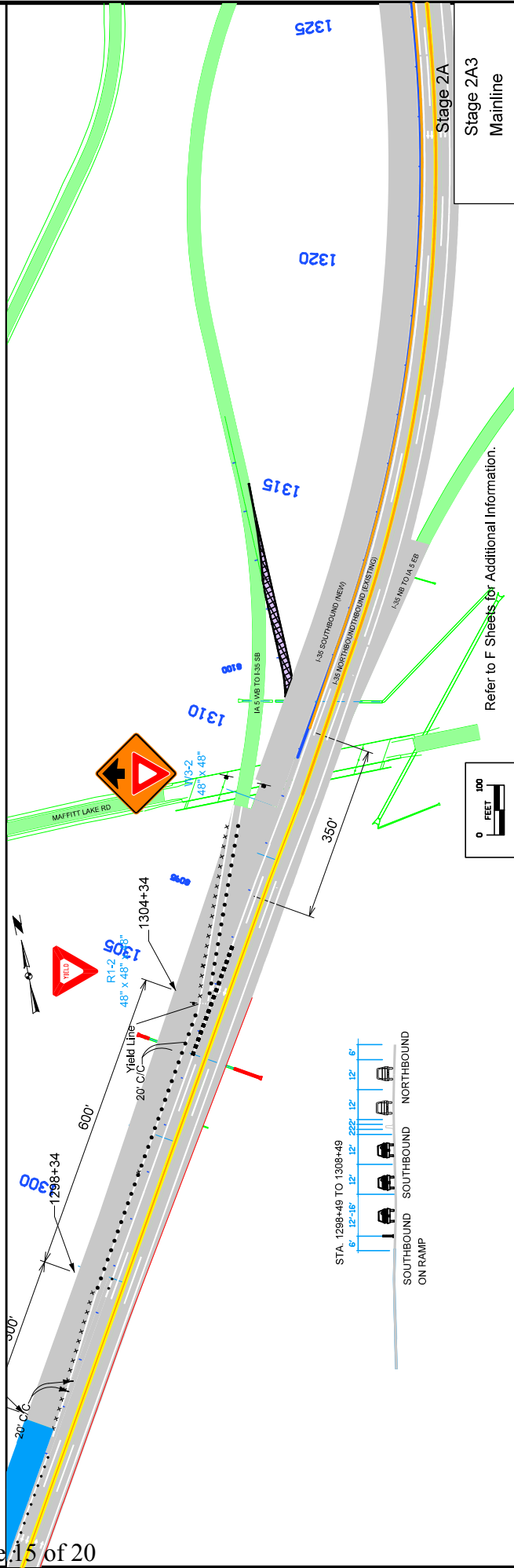
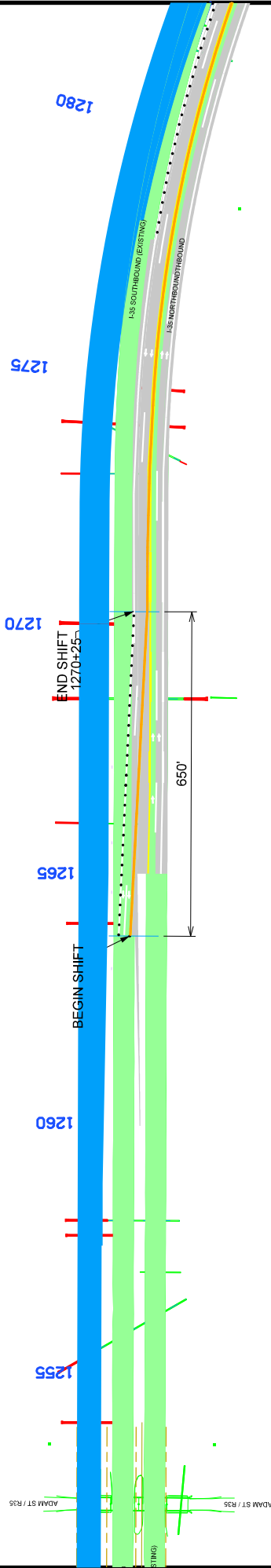


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|-----------|-------|-------------------|--------------------|----------------|------------------------|--------------|------|
| ENCLIN | DATE | DESIGN TEAM | WARREN/POLK COUNTY | PROJECT NUMBER | IM-035-2(336)65--13-91 | SHEET NUMBER | J.25 |
| 3/15/2013 | asmyn | Jia Hoakina Smyth | | | | | |

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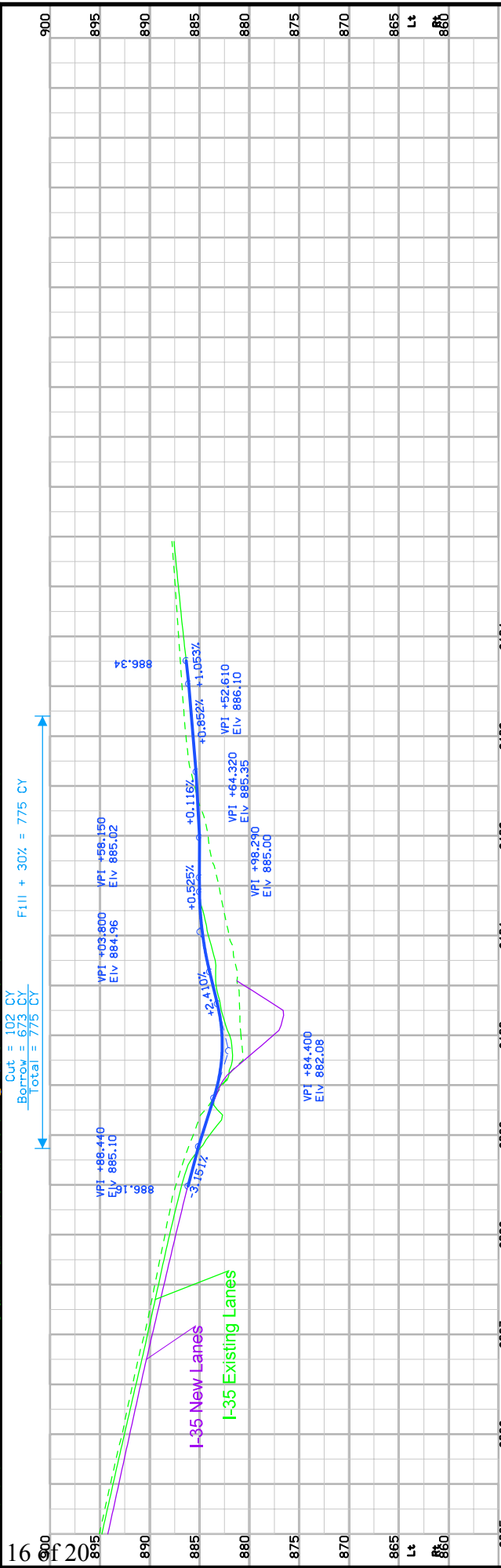
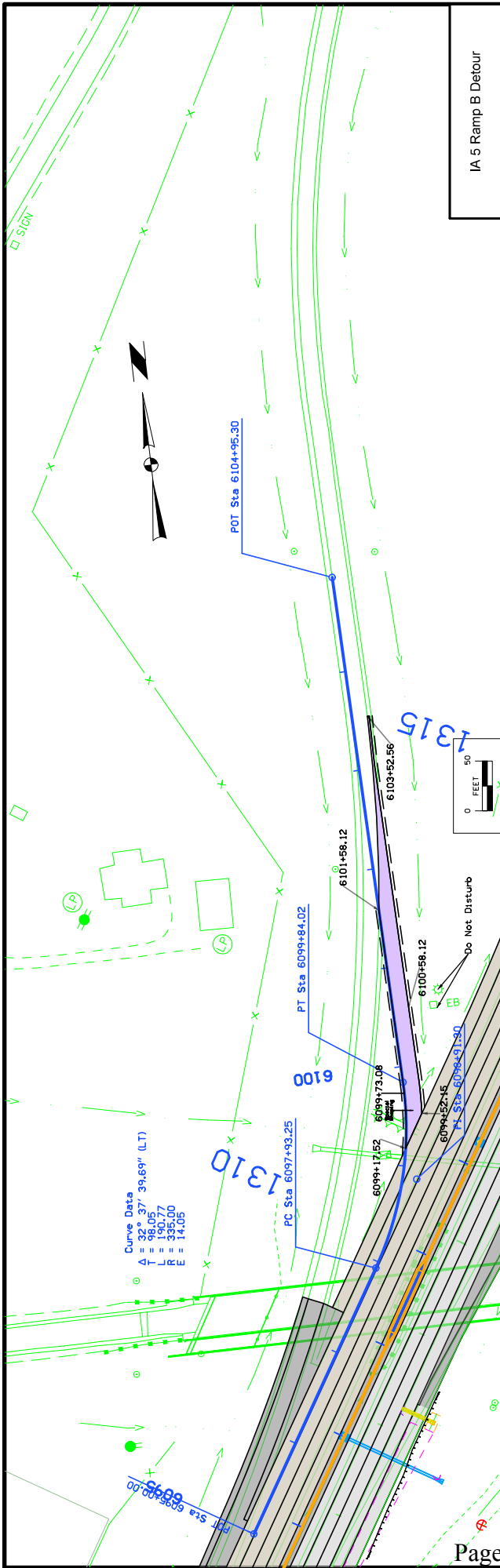
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| ENCL 01 | DATE | DESIGN TEAM | COUNTY | PROJECT NUMBER | SHEET NUMBER |
| C | 3/15/2013 | Jia Hoakina Smyth | WARREN/POLK | IM-035-2(336)65--13-91 | J.26 |
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| Stage 2A Stage 2A3 Mainline | | | | | |



STA. 1298+49 TO 1308+49
 6' 12'-16' 12' 12' 22'-12' 12' 6'

SOUTHBOUND SOUTHBOUND NORTHBOUND ON RAMP

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| | | | | | | | | | |
|---------|----------|-------------|-------------------|--------------------|----------------|------------------------|--------------|------|------|
| 6095 | 6096 | 6097 | 6098 | 6099 | 6100 | 6101 | 6102 | 6103 | 6104 |
| ENGLISH | IDWA DOT | DESIGN TEAM | Jia\Hoakina\Smyth | WARREN/POLK COUNTY | PROJECT NUMBER | IM-035-2(336)65--13-91 | SHEET NUMBER | F.1 | |

PT Sta 6099+84.02

6100

6101+58.12

6099+17.52
(883.79)

6099+73.08
Special Shaping

(883.86)

3.0%
16.0'

3.0%
16.0'

Uniform Transition

Constant 4.0% Slope

6103+52.56

(886.86)

6099+52.15
(882.81)

(884.34) 6100+58.12

(885.66)

(885.02)

PI Sta 6098+91.30

Staking Detail
IA 5 Ramp B Detour

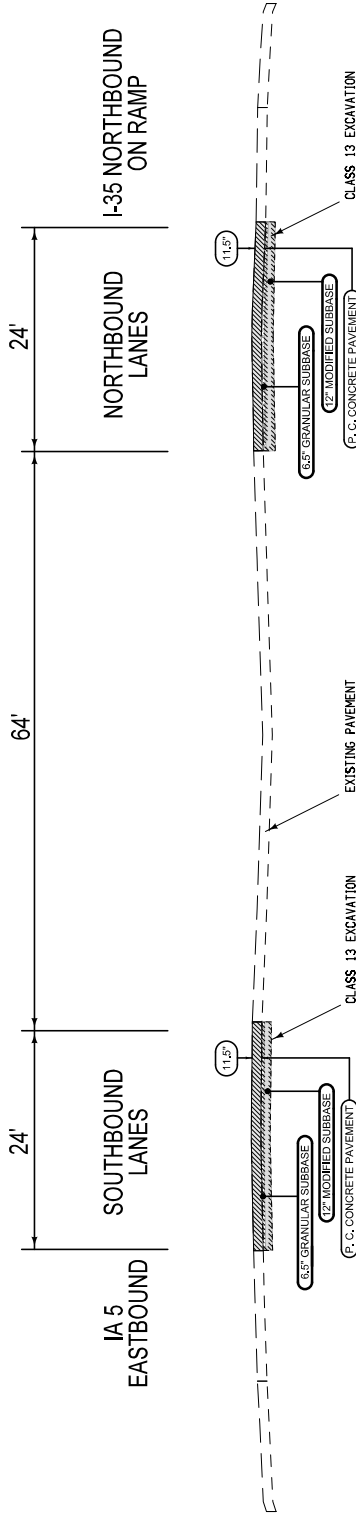
Do Not Disturb

ALIGNMENT COORDINATES

101-16
10-20-09

| Name | Location | Station | Point on Tangent | | Begin Spiral | | Simple Curve PI or Master PI of SCS | | End Curve | | End Spiral | |
|--------|-------------|---------|------------------|-------------|--------------|--------------|-------------------------------------|--------------|--------------|------------|--------------|--------------|
| | | | Y (Northing) | X (Easting) | Y (Northing) | X (Easting) | Station | Y (Northing) | X (Easting) | Station | Y (Northing) | X (Easting) |
| Detour | IA 5 Ramp B | | | | | | | | | | | |
| 2003 | | | | | 6096+00.00 | 1,564,968.48 | 6096+00.00 | 553,517.36 | 1,564,968.48 | 6099+84.02 | 553,949.24 | 1,565,160.55 |
| 2001 | | | | | 6098+91.30 | 1,565,162.59 | 6098+91.30 | 553,851.21 | 1,565,162.59 | | | |
| 2000 | | | | | 6104+96.30 | 1,565,149.96 | 6104+96.30 | 554,460.41 | 1,565,149.96 | | | |

INLAY
SPECIAL



12" PCC INLAY
STA. 1345+30 TO STA. 1356+10.97

SHEET NUMBER

B.4

PROJECT NUMBER

IM-035-2(336)65--13-91

COUNTY

WARREN/POLK

DESIGN TEAM

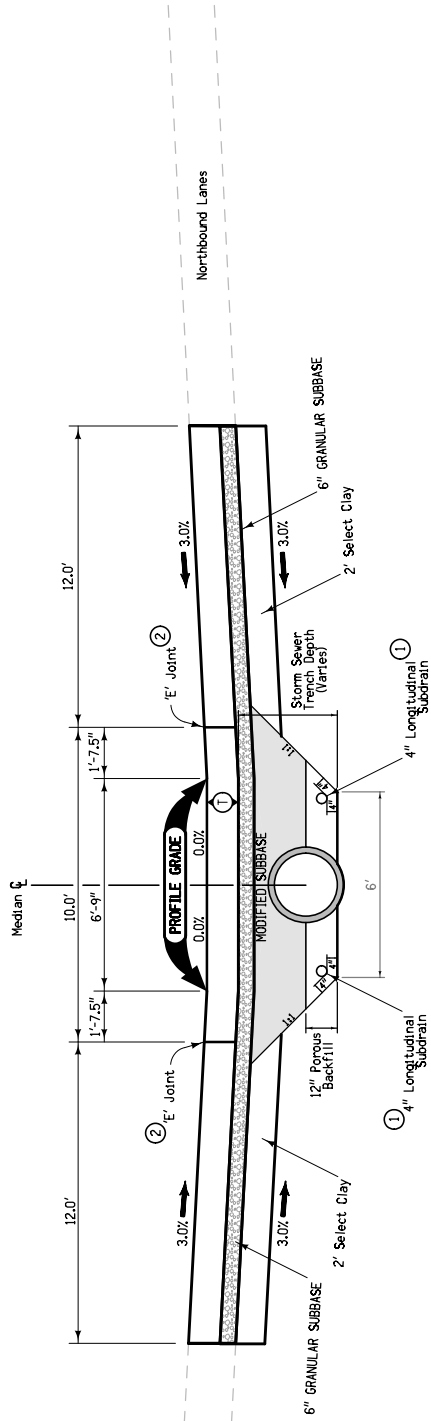
Jia\Hoskins\Smyth

DATE

3/15/2013

11:00:22 AM

MED-2
MODIFIED



WITH STORM SEWER TRENCH

TYPICAL MEDIAN CROSS SECTION
STORM SEWER TRENCH AND
SUBDRAIN PLACEMENT

| Road Identification | Location | Station To Station | Inches |
|---------------------|----------|--------------------|--------|
| P-35 | 1293+00 | 1302+00 | 11.5 |
| P-35 | 1305+00 | 1308+00 | 11.2 |
| P-35 | 1311+25 | 1325+00 | 11.2 |
| P-35 | 1332+00 | 1344+00 | 11.5 |

Notes:

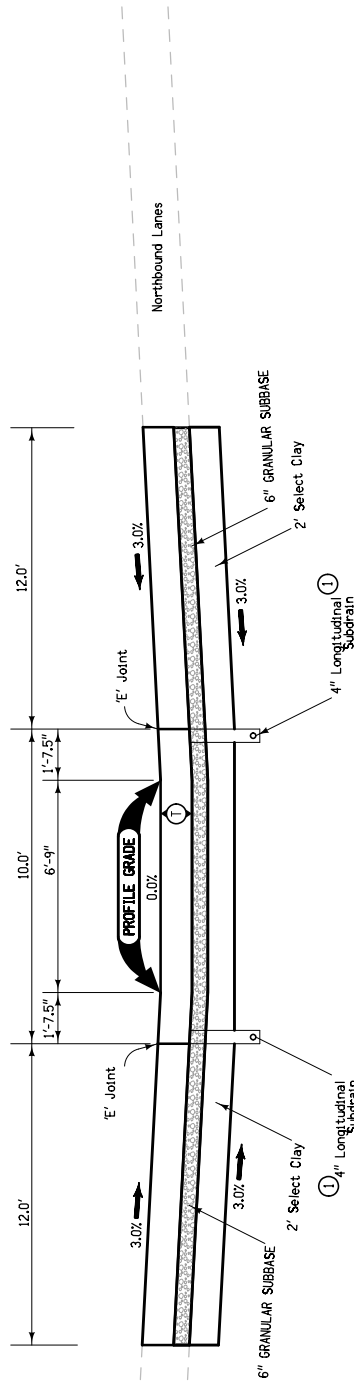
Transverse joints: C at 20' spacing

- ① Refer to Tab. 104-9 for details of subdrain installation.
- ② See Standard Road Plan PV-1

*PAVEMENT SMOOTHNESS REQUIREMENTS.

Profile index evaluation & correction will not be required in this area. Evaluation & correction of 1/2" bumps and dips only.

MED-3
SPECIAL



WITHOUT STORM SEWER TRENCH

TYPICAL MEDIAN CROSS SECTION
WITHOUT STORM SEWER TRENCH

| Road Identification | Location | Station To Station | Inches |
|---------------------|----------|--------------------|--------|
| P-35 | 1285+65 | 1293+00 | 11.5 |
| P-35 | 1302+00 | 1305+00 | 11.5 |
| P-35 | 1308+00 | 1311+25 | 11.5 |
| P-35 | 1325+00 | 1332+00 | 11.5 |
| P-35 | 1344+00 | 1345+30 | 11.5 |

Notes:

Transverse joints: C at 20' spacing

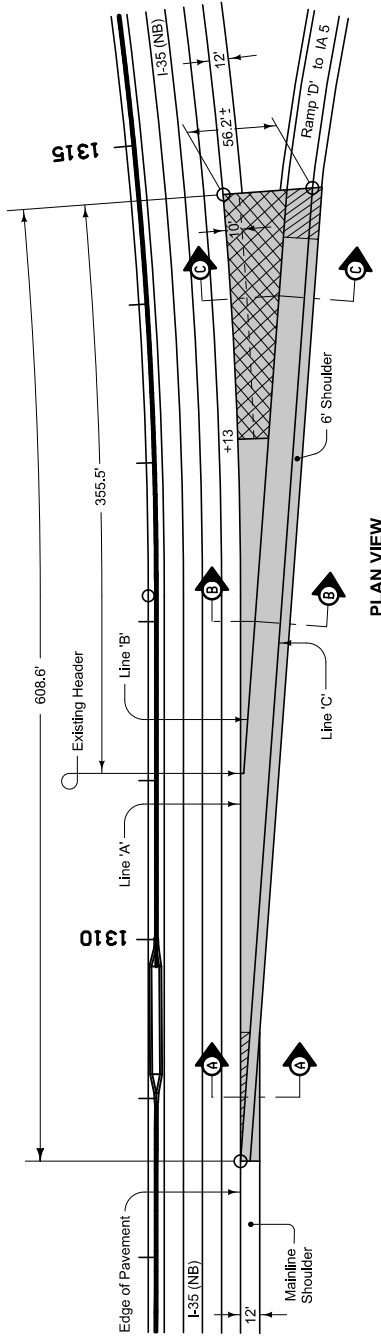
- ① Refer to Tab. 104-9 for details of subdrain installation.

*PAVEMENT SMOOTHNESS REQUIREMENTS.

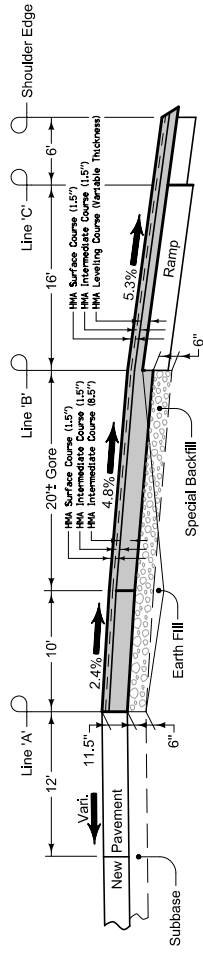
Profile index evaluation & correction will not be required in this area. Evaluation & correction of 1/2" bumps and dips only.

NOTES:

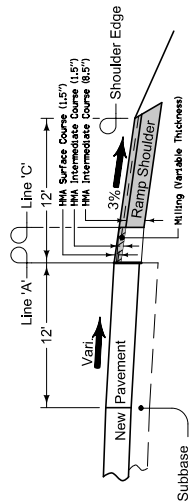
Refer to Sheet K.14 for Staking Details.
Refer to Other Sheets for Staging Details.



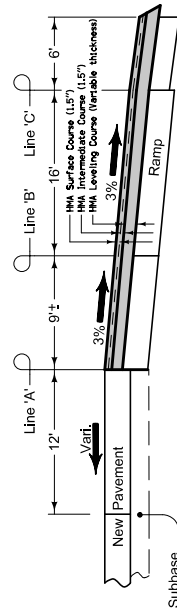
PLAN VIEW



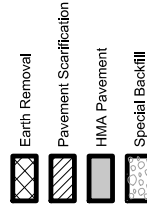
SECTION C-C



SECTION A-A



SECTION B-B



TABULATION OF LEVELING COURSES

| STATION | From | To | Average Thickness (Inches) | HMA PAVEMENT | Amount (Tons) |
|---------|-------------|-------------|----------------------------|--------------|---------------|
| | 1309+41.50± | 1313+13.00± | 4.0 | | 96 (1) |
| | 1313+13.00± | 1314+65.50± | 3.0 | | 62 (1) |

(1) Quantity includes 5% for Irregularities.

SPECIAL DETAIL SHEET SDS-1

CONSTRUCTION DETAILS FOR RAMP TAPER
I-35 NB TO IA 5 EB

SHEET NUMBER **U.10**

PROJECT NUMBER **IM-035-2(336)65--13-91**

COUNTY **WARREN/POLK**

DESIGN TEAM **JJA Hoskins Smyth**
ENGLISH IDMA DOT dmaach
3/15/2013
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